



Correspondence Between Staff and Applicant
Approval Letter



August 29, 2019

George Pasquel III
Withey Morris, PLC
2525 E Arizona Biltmore Cir A-212
Phoenix, AZ 85016

Re: 1155-PA-2014
7-ZN-2015#2
Marquee

Dear George Pasquel III,

This is to advise you that the case referenced above was approved at the August 27, 2019 City Council meeting. The City Council related documents may be obtained from the City Clerk's office located at 3939 N. Drinkwater Blvd. Scottsdale, AZ 85251 or by entering the document number through the city website @ <https://eservices.scottsdaleaz.gov/cityclerk/DocumentSearch>

Please remove the red hearing sign as soon as possible. If you have any questions, please contact me at 480-312-2258.

Sincerely,

Bryan Cluff
Senior Planner

MARQUEE PROJECT
(7-ZN-2015#2 AND 8-DA-2018)
Response to 2nd Review Comments

Responses are provided in **RED CAPS BELOW**

Zoning:

1. As identified in the 1st review letter, Zoning Ordinance Section 6.1304.A.3.b and c., within the Criteria for a PBD Overlay District Application in the Type-3 Area, requires that the building form be in conformance with the Downtown Plan Urban Design & Architectural Design Guidelines (DUDAG) and shall reflect the planned character of development within which the development project will be located. The DUDAG specify that the apparent size and bulk of larger architectural form of buildings are to be reduced, particularly adjacent to streets, with taller vertical planes limited to the interior of the development. Furthermore, the apparent size and bulk should be designed as compositions of smaller parts that reduce a building's perceived height and bulk by dividing the building mass into smaller-scale components and stepping building height back away from the street as the building height increases.

The northern (Shoeman Lane) building face is sheer vertical with no apparent articulation or stepback in building mass. Although the west elevation (Scottsdale Road) has some amount of stepback in the building plane, it falls short of design guideline expectations. The overall building massing, as proposed, does not appear to achieve the standard of high-quality architecture and design that is imperative in the Downtown area, and may not have staff support moving forward. Please revise the building elevations in accordance with the above requirements, dividing the building mass into smaller-scale components and stepping building height back away from the street as the building height increases.

PLEASE REFER TO CURRENT DESIGN EXHIBITS SUBMITTED WITH THIS RESPONSE LETTER. THE DESIGN TEAM FEELS IT HAS PROVIDED A DESIGN THAT REDUCES THE BUILDING MASS INTO SMALLER COMPOSITIONS TO REDUCE THE PERCEIVED HEIGHT AND MASS. THE NORTH ELEVATION INTRODUCES PORTALS THAT PROJECT BEYOND THE INITIAL PLANE OF THE STRUCTURE.

PLEASE ALSO REFER TO THE SECTIONS AND RENDERINGS PROVIDED ILLUSTRATING THE STEP BACK OF THE WEST FAÇADE ALONG SCOTTSDALE ROAD. THE DESIGN TEAM FEELS THIS ADDRESSES THE REQUIREMENTS TO REDUCE THE BUILDING MASSING INTO SMALLER COMPONENTS.

2. As identified in the 1st review letter, the DUDAG discourage buildings that have a predominately glass façade. The extensive use of glass on the north and west building elevations will increase reflected heat, sunlight, and glare, potentially creating a negative impact on surrounding buildings and pedestrian areas. In addition, the upper level patios on the west elevation are highly vulnerable to direct and reflected sun exposure. This design is not considerate of the harsh desert climate of the region, or the architectural character and context of the area, and may not have staff support moving forward. Please revise the design of the building elevations to provide more solid wall area and/or alternative materials resulting in less glass and incorporate architectural shading techniques including but not limited to louvers, large overhangs, and significant recesses. (Section C2 and C3 of the DUDAG and SDGOD, Local Influence on Design 3 and 4, Scale and Proportion 19 and the Scottsdale Sensitive Design Principle 9).

PLEASE REFER TO CURRENT DESIGN EXHIBITS SUBMITTED WITH THIS RESPONSE LETTER. THE DESIGN TEAM HAS TAKEN EFFORTS TO ADDRESS CONCERNS OF HEAT GAIN, REFLECTED HEAT AND GLARE THROUGH COMMON DESIGN TECHNIQUES UTILIZED IN THE REGION. THIS DESIGN APPROACH INCLUDES REDUCED GLAZING AT THE EAST AND WEST FACADES (LESS THAN 50% GLAZING AT EACH ELEVATION) AS WELL AS HORIZONTAL SHADE CONTROL DEVICES AND DEEPER OVERHANGS ON THE SOUTH FAÇADE. MOST OF THE BUILDING GLASS DOES OCCUR ON THE NORTH FAÇADE, WHICH IS BEST PRACTICE TO MAXIMIZE VIEWS AND DAYLIGHT, WHILE AVOIDING NEGATIVE IMPACTS FROM SUN EXPOSURE.

3. On the building elevations and/or additionally provided building sections, please add building stepback lines for the proposed building, existing entitlements, and Downtown ordinance requirements to illustrate the proposed building setbacks and stepbacks relative to the existing entitlements and standard Downtown District requirements in accordance with Zoning Ordinance Section 5.3006.

PLEASE REFER TO CURRENT DESIGN EXHIBITS SUBMITTED WITH THIS RESPONSE LETTER. THESE DIAGRAMS HAVE BEEN INCLUDED TO ILLUSTRATE THE DESIGN TEAM'S EFFORT TO MEET THE REQUIREMENTS DIRECTED BY THE CITY OF SCOTTSDALE WITH REGARD TO BUILDING SETBACKS AND STEPBACKS.

4. Paragraph 5.5 of the approved development agreement states that the project will not receive any parking credits for the spaces removed from Shoeman Lane. The new development agreement adds language suggesting the possibility of the property receiving parking credits for on-street spaces that remain on Shoeman Lane, if the zoning ordinance permits. Since there is no longer and P-3 zoning on the site, the zoning ordinance does not permit any parking credits. Please remove this language from the agreement.

THIS LANGUAGE WAS ADDED TO PROTECT THE PROPERTY SHOULD

THE ORDINANCE CHANGE IN THE FUTURE TO ALLOW ON-STREET PARKING SPACES TO BE COUNTED TOWARDS PARKING CALCULATIONS (AS CURRENTLY OCCURS OUTSIDE OF THE DOWNTOWN AREA). NONETHELESS, THE LANGUAGE HAS BEEN REMOVED.

5. Please revise the tabulations under the "Public Benefits" heading on page 23 of the project narrative so the requested bonus calculations accurately reflect the requirements of Zoning Ordinance Section 7.1200.D. The requested bonus height is 72 feet, due to the proposed mechanical equipment. The requested GFAR bonus appears to be 138,702.5 square feet based on existing square footage and current zoning requirements. Please make sure the language in the development agreement is consistent with these numbers.

THE LANGUAGE IN THE DEVELOPMENT AGREEMENT HAS BEEN UPDATED TO REFLECT THE MOST UP TO DATE SQUARE FEET AND TO REFLECT THE HEIGHT OF THE MECHANICAL EQUIPMENT.

General Plan:

6. Regarding comment #1 from the 1st review letter, although the provision of open space is more explicitly expressed in the most recent submittal, the proposal has now been adjusted to incorporate a larger building mass that extends 150' in height, whereas the previous proposal requested a maximum 120' in height. Please consider providing additional public open space at ground-level that more closely aligns with the additional height and development bonuses being sought through this application.

PER THE ORDINANCE, OPEN SPACE IS NOT REQUIRED AT THIS LOCATION. NONETHELESS, THE PROJECT PROVIDES OVER 12,000 SQUARE FEET OF OPEN SPACE ALONG BOTH STREET FRONTAGES, WITH A SUBSTANTIAL PORTION OF THAT OPEN SPACE SHADED FOR A COMFORTABLE EXPERIENCE FOR OFFICE EMPLOYEES, VISITORS, RESTAURANT PATRONS AND PASSING PEDESTRIANS.

Site Design:

7. Please provide a formal response and revisions as necessary in response to the March 6, 2019 email regarding preliminary trash service design, attached to this letter for reference.

BASED UPON COORDINATION WITH SCOTTSDALE SOLID WASTE AND ENGINEERING MANAGER, ELIANA HAYES, IT IS ACCEPTABLE (AND IS OUR INTENT) TO UTILIZE THE EXISTING EXCESS COMPACTOR FACILITIES WITHIN THE GALLERIA TRUCK DOCK TO SUPPORT THE NEW

MARQUEE OFFICE BUILDING. THIS ASSUMES THAT A CROSS ACCESS EASEMENTS WILL BE PROVIDED AND THE EXISTING EQUIPMENT HAS THE CAPACITY TO SERVE BOTH DEVELOPMENTS.

IN SUMMARY, THERE ARE THREE EXISTING 30 CY COMPACTORS ONSITE. TWO ARE FOR TRASH AND ANOTHER IS FOR RECYCLING. THE TOTAL GALLERIA BUILDING OCCUPIES LESS THAN 600,000 SF. THAT AREA IS THEN DIVIDED BY 20,000 (PER DSPM) TO DETERMINE HOW MANY 4CY BINS ARE REQUIRED. THAT EQUATES TO THIRTY 4-CY BINS OR ONE 30 CY COMPACTOR FOR THE EXISTING GALLERIA.

MARQUEE PROPOSES ANOTHER 268,000 SF /20,000 = 13.4 CY. AS SUCH, THE EXISTING SECOND 30 CY COMPACTOR IS MORE THAN SUFFICIENT TO SERVE THE NEW DEVELOPMENT. IN ANY EVENT, PICKUP CURRENTLY IS ONLY ONCE PER WEEK AND IF REQUIRED, ADDITIONAL PICKUPS WOULD BE AN OPTION.

OPERATIONALLY, BOTH DEVELOPMENTS WILL CONTINUE TO OPERATE ESSENTIALLY AS IT DOES TODAY WITH TRUCKS BACKING UP FROM SHOEMAN LANE AND PULLING STRAIGHT BACK OUT.

BASED UPON THE ABOVE SUMMARY AND CONCURRENCE BY ELIANA HAYES ON 5-20-19, TRASH AND RECYCLING HAS BEEN SUFFICIENTLY ACCOUNTED FOR FOR THE MARQUEE OFFICE BUILDING WITHIN THE EXISTING GALLERIA TRUCK DOCK.

8. Please provide additional information including dimensions and clearance information for bike rack locations, demonstrating the appropriate clearance is provided without conflicting with the walkways or parallel parking areas, in accordance with Zoning Ordinance Section 9.106.A.2.

PLEASE REFER TO CURRENT DESIGN EXHIBITS SUBMITTED WITH THIS RESPONSE LETTER. THE DESIGN TEAM HAS CONFIRMED NO CONFLICTS EXIST WITH PROPOSED BICYCLE PARKING AREA AND ANY WALKWAYS/PARALLEL PARKING AREAS. IT HAS ALSO BEEN CONFIRMED THAT THE BIKE PARKING AREA IS LOCATED WITHIN 50' A BUILDING ENTRANCE.

9. Please revise the site plan or provide a supplemental plan to show dimensions of the right-of-way and proposed improvements on Shoeman Lane, and include a dimensioned street cross-section, in accordance with the Plan & Report Requirements for Development applications.

PLEASE REFER TO CURRENT DESIGN EXHIBITS SUBMITTED WITH THIS RESPONSE LETTER. DIMENSIONS TO THE ROW AND PROPOSED

IMPROVEMENTS ARE ILLUSTRATED THROUGH CROSS SECTIONS PROVIDED THROUGH SHOEMAN LANE.

Site:

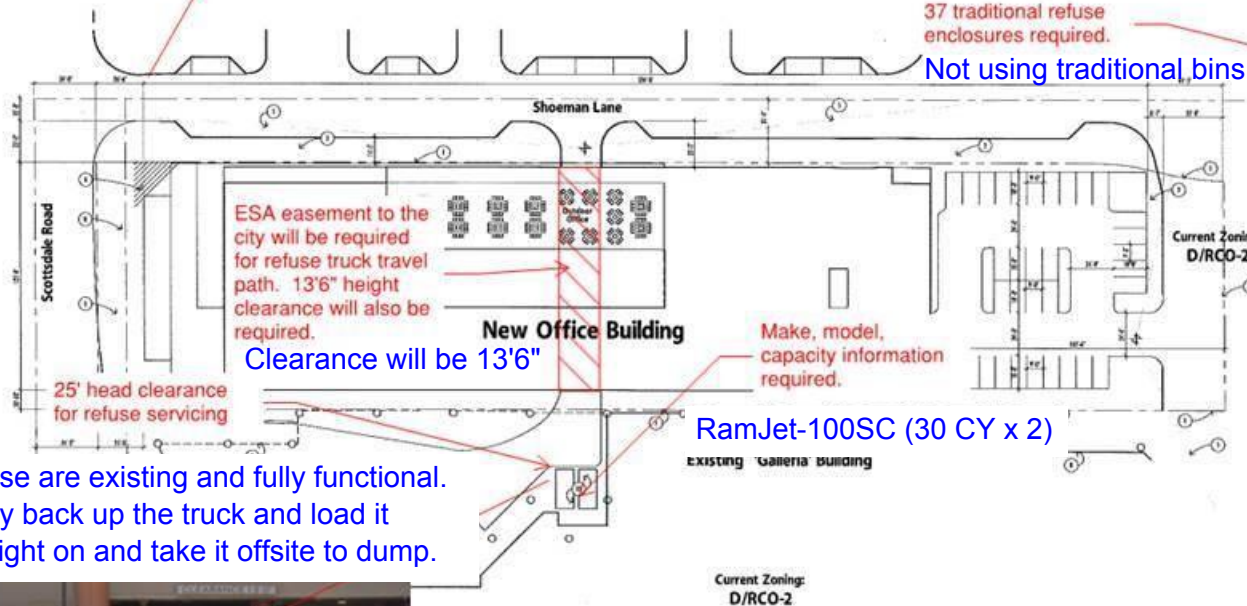
10. Please provide specific setback dimensions on Scottsdale Road and Shoeman Lane, for the building face (outside of columns) and the building face interior to the arcade, from back of curb, both inside the parallel parking spaces and at the outside curb.

PLEASE REFER TO CURRENT DESIGN EXHIBITS SUBMITTED WITH THIS RESPONSE LETTER. SETBACK DIMENSIONS ARE ILLUSTRATED THROUGH CROSS SECTIONS PROVIDED AT BOTH SCOTTSDALE ROAD AND SHOEMAN LANE.

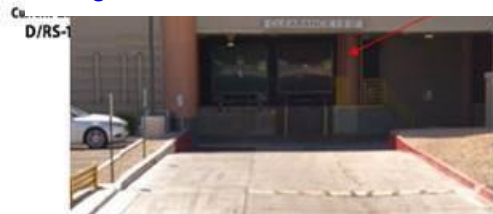
This has not been addressed: Provide the street cross section, sidewalk width, pavement striping, and parking configuration for Shoeman Lane from Scottsdale Road to Buckboard Trail, both sides of the street. See landscape / Site plans. Let us know if you need a civil plan as well.



Current Zoning:
C-2



These are existing and fully functional. They back up the truck and load it straight on and take it offsite to dump.



Current Zoning:
D/RS-1

Current Zoning:
D/RCO-2

KEYNOTES

1. PROPERTY LINE
2. CONCRETE SIDEWALK, TYP.
3. BIKE PARKING
4. PARKING GARAGE
5. SITE DISTANCE TRIANGLE
6. TRAFFIC SAFETY TRIANGLE
7. NEW DEDICATED R.O.W. FOR SCOTTSDALE RD.
8. GALLERIA FOOTPRINT
9. GALLERIA SERVICE/REFUSE AREA
10. TRASH COMPACTORS

Site Plan

| | |
|-------------------------------------|--|
| Address | 4419 N. Scottsdale Rd. Scottsdale, AZ 85251 |
| APN# | 173-41-007A; 173-41-011; 173-41-010; 173-41-264 |
| Existing Zoning | D/DMU-2, PBD, DO |
| Proposed Zoning | D/DMU-3, PBD, DO |
| Sub District | Downtown Multiple Use Type 2 |
| 'Galleria' Site Area | 6.29 acres net (274,082sf) 6.68 acres gross (291,023sf) |
| Office Site Area | 1.93 acres net (83,870sf) 2.63 acres gross (114,442sf) |
| Total Site Area | 8.22 acres net (357,952sf) 9.31 acres gross (405,465sf) |
| 'Galleria' Building Area (existing) | 475,000sf |
| Office / Retail Building Area (new) | 271,900sf |
| Total Area (excluding garages) | 746,900sf |
| Height Allowed | 84'-0" |
| Height Provided | 150'-0" (top of parapet) 156'-0" (top of mech screen) |
| GFAR Allowed | 1.40 (4.00 with bonus) |
| GFAR Provided | 2.30 (total gross building) |
| Building Setback | 25'-0" from existing curb (not required) |
| Open Space | |
| 'Galleria' Parking Required | 1,584 spaces (1/300sf) |
| Office / Retail Parking Required | 906 spaces (1/300sf) |
| Total Parking Required | 2,490 spaces |
| Parking Provided | 2,058 spaces (existing Galleria) 948 spaces (new Office) 19 spaces on Shoeman Lane |
| Bicycle Parking Required | 3,025 spaces total |
| Bicycle Parking Provided | 91 spaces (1 per 10 spaces req.) 91 spaces (see Office First Floor for locations) |

APPLICANT
Stockdale Capital Partners, LLC
4501 n. Scottsdale Road, #201
Scottsdale, AZ. 85251
p: 602-678-8888
contact: Boyce O'Brien
bobrienstockdalecapital.com

ARCHITECT
DAVIS
74 E. Rio Salado Parkway
Suite 200
Tempe, AZ 85281
p: 480-638-1100
contact: Mike Edwards
medwards@thedavisexperience.com

SCALE: 1" = 30'-0" 1" 2" 3" 4" 5" 6" 7" 8" 9" 10" 11" 12" 13" 14" 15" 16" 17" 18" 19" 20" 21" 22" 23" 24" 25" 26" 27" 28" 29" 30" 31" 32" 33" 34" 35" 36" 37" 38" 39" 40" 41" 42" 43" 44" 45" 46" 47" 48" 49" 50" 51" 52" 53" 54" 55" 56" 57" 58" 59" 60" 61" 62" 63" 64" 65" 66" 67" 68" 69" 70" 71" 72" 73" 74" 75" 76" 77" 78" 79" 80" 81" 82" 83" 84" 85" 86" 87" 88" 89" 90" 91" 92" 93" 94" 95" 96" 97" 98" 99" 100"

Darrel E. Wood, P.E., R.L.S.
Ashok C. Patel, P.E., R.L.S., CFM
Michael T. Young, P.E., LEED AP
James S. Campbell, P.E., LEED GA
Thomas R. Gettings, R.L.S.
Darin L. Moore, P.E., LEED GA
Jeffrey R. Minch, P.E., CFM
Robert D. Gofonia, P.E., R.L.S.

February 22, 2019

Mr. Rezaur Rahman
City of Scottsdale
7447 East Indian School Road, Suite 5
Scottsdale, AZ 85281

Re: **Marquee**
Existing Galleria Corporate Centre Utility Services
WP# 174722.01

Dear Mr. Rahman:

Wood, Patel & Associates, Inc. (Wood/Patel), at the request of Stockdale Capital Partners, LLC (Developer), has prepared this letter in support of the rezoning case 7-ZN-2015#2 for Marquee (Project). The purpose of this letter is to discuss the existing domestic water, sanitary sewer and storm drain improvements serving the Galleria Corporate Centre (Galleria) Building that currently reside within the Project limits, and how service for each of these utilities is planned to be maintained by the Project. Below is a description of the Project limits as well as the plan for maintaining each of these Galleria utility services. The Galleria utility services described are not intended to be adversely impacted by the Project's development.

Project Limits:

The Project is planned to redevelop the existing property at the southeast corner of Scottsdale Road and Shoeman Lane in Scottsdale, Arizona. The project limits are described as Scottsdale Road to the west, Shoeman Lane to the north, Galleria Corporate Centre (Galleria) Building to the south, and an existing private access way serving the Galleria parking structure to the east.

Galleria Domestic Water Service:

Domestic water service for the Galleria building is currently provided by a 6-inch water service line extending from the existing 8-inch public waterline in Scottsdale road. The water service extends along the southern boundary of the site to a 6-inch meter vault. From the meter vault the 6" water service splits into two (2) 4-inch backflow prevention devices and then manifolds back into a 6-inch water service that continues along the southern property line before turning south into the Galleria building at the existing truck dock. This water service resides within an existing 8-foot-wide irrigation and public utility easement (I.U.E.) which was dedicated in the 1950's as part of the Shoeman Tract.

The Project intends to relocate the existing water service for the Galleria. A new 6-inch water meter vault is planned to be installed along the southern edge of Shoeman Lane with connection to the existing 6-inch public water line within Shoeman Lane. From the new meter vault a new 6-inch water service line (with backflow prevention device) will extend south through a proposed private access drive to the existing Galleria truck dock entrance where the existing water service enters the building. A public water line easement is planned to be dedicated around the new meter vault contiguous with the public right-of-way



(should there not be sufficient room for the vault within the public right-of-way). A private utility easement is planned to be dedicated along the new Galleria water service to the benefit of Galleria for access and maintenance. Construction is planned to be phased such that the new Galleria water service is operational prior to removing or abandoning the existing water service line.

Galleria Sanitary Sewer Service:

A portion of the sanitary sewer service for the Galleria Building is currently served by an 8-inch public sewer line that runs west along the Project's southern property line to an existing manhole in the Scottsdale Road right-of-way. This existing sewer line resides within the aforementioned 8-foot-wide I.U.E.

The Project intends to replace the existing public sewer line serving the Galleria Building with a private sewer line along the same alignment. The I.U.E. is planned to be abandoned and a private utility easement is planned to be dedicated along the new Galleria sewer service to the benefit of Galleria for access and maintenance. Construction is planned to be phased such that the new Galleria sewer service is operational prior to removing or abandoning the existing sewer service line.

Galleria Storm Drain:

An existing 24-inch public storm drain line runs east along the southern property line to an existing storm drain manhole within the private access drive on the east side of the project. From there the storm drain line increases to a 30-inch storm drain and continues south through the Galleria Property before connecting to the 30-inch storm drain in Stetson Drive. This storm drain resides within the aforementioned I.U.E. and serves roof drain connections from the Galleria building as well as surface catch basins from the existing onsite parking lots.

The Project intends to maintain the existing 24-inch storm drain in place as a private storm drain line for the benefit of both the Galleria and the Marquee. As previously mentioned, the I.U.E. is planned to be abandoned. A cross-drainage easement is planned to be dedicated over the storm drain line to the benefit of Galleria for access and maintenance.

Thank you for your consideration in reviewing this letter. Please contact us should you have any questions regarding this letter.

Sincerely,

Wood, Patel & Associates, Inc.

Derek C. Nichols, P.E.
Project Manager



cc: Mr. Boyce O'Brien – Stockdale Capital Partners, LLC

DCN/se

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7-ZN-2015#2
02/26/2019